

PLANNING COMMISSION REPORT



MEETING DATE: November 10, 2004

ITEM NO. _____

GOAL: Coordinate Planning to Balance Infrastructure

SUBJECT

W Hotel Scottsdale - 22-ZN-2004 & 19-AB-2004

REQUEST

Request:

1. To rezone from Highway Commercial, Downtown Overlay District (C-3 DO), Central Business, Parking, Downtown Overlay District (C-2 P-3 DO), and Automobile Parking, Downtown Overlay District (P-2 DO) to Downtown/Office Residential District Type 2, Planned Block Development, Downtown Overlay (D/OR-2 PBD DO) with amended development standards on approximately 2.2 acre parcel located on the southeast corner of Brown Avenue and Camelback Road.
2. To abandon alley located south of Camelback Road, north of Shoeman Lane, and located between Brown Avenue and Buckboard Trail.

Key Items for Consideration:

- Rezoning conforms to the General Plan's Downtown Land Use Plan.
- This will create a mixed-use project in downtown, supporting the live, play, and work environment.
- The proposed project will add a new hotel to downtown and compliment the surrounding entertainment uses.
- The Development Review Board supports the proposed amended development standards by a vote of 6-0.
- Traffic generated by the proposal is similar to what currently can be developed on the site and will not exceed the capacities of affected streets.

Related Policies, References:

- Downtown Land Use Plan, which guides zoning, uses, and development types.
- Downtown Design and Architectural Guidelines, which describes goals and objectives of the built environment.

OWNER

Downtown Scottsdale Development LLC
480-748-8888

APPLICANT CONTACT

John Berry
Beus Gilbert P L L C
480-429-3000



| | |
|----------------------|--|
| LOCATION | 7243 E Camelback Rd |
| BACKGROUND | <p>Zoning.</p> <p>The subject property currently has multiple zoning districts. The west half of the property is zoned Highway Commercial, Downtown Overlay District (C-3 DO), the former alley bisecting the property is zoned Automobile Parking, Downtown Overlay District (P-2 DO), and the east half of the property is zoned Central Business, Parking, Downtown Overlay District (C-2 P-3 DO). These zoning districts allow for a wide variety of commercial uses to serve the community, as well as limited residential and parking opportunities.</p> <p>Downtown Plan.</p> <p>The General Plan's Downtown Land Use Plan designates this area as Office Residential Type 2, with the primary land uses planned for this area being office, residential, and hotels. To develop a strong mixed-use character, development flexibility for land assembly, shared parking facilities, floor area, and height adjustments is allowed. The Type 2 Development designation places emphasis on intermediate-scaled structures having a good pedestrian environment with efficient auto access. Open space is encouraged for privacy and amenities vital to residential/hotel uses, and flexibility toward building height is allowed to encourage quality design for mixed-use projects.</p> <p>Context.</p> <p>This property is located along Camelback Road between Brown Avenue and Buckboard Trail, is surrounded by a variety of land uses, and is located near an active entertainment district. To the north of Camelback Road is an electrical substation, multi-family residential, and further to the northeast is an approved hotel and single-family homes. To the east are a variety of offices, restaurants, and nightclubs. To the south and west are office buildings, including the Galleria Corporate Centre to the south. Multi-use downtown projects are planned or under construction further to the west at the intersection of Camelback Road and Scottsdale Road that will tie into the existing canal waterfront and the existing Fashion Square Mall.</p> |
| APPLICANT'S PROPOSAL | <p>Goal/Purpose of Request.</p> <p>The applicant has four requests with this application:</p> <ol style="list-style-type: none">1). To rezone the 2.2 +/-acre parcel to Downtown Office Residential Type 2 District Downtown Overlay (D/OR-2 DO) consistent with the Downtown Land Use Plan.2). To apply the Planned Block Development Overlay (PBD) to the 2.2 +/- acre parcel.3). To amend the Downtown Development Standards within the D/OR-2 PBD DO district.4.). To abandon an existing alley located south of Camelback Road, north of Shoeman Lane, and located between Brown Avenue and Buckboard Trail. <p>Development Information:</p> <p>The applicant proposes to build a 72-foot tall (7-level) mixed-use project consisting of a hotel, some residential units, restaurant and bar, and some minor retail.</p> |

- Parcel Size: 2.2+/- acres
- Proposed use: Mixed used (hotel, residential, restaurant, retail)
- Building Height: 72 feet (7 levels)
- Number of Units: 225 hotel rooms, 25 condominiums
- Proposed Square Footage: 200,000+/- square feet
- Parking: Underground parking, approximately 217 spaces

Planned Block Development (PBD) Standards Amendment:

The Downtown development standards and architectural design guidelines are intended to assure that developments are designed to fit into the established urban pattern of Downtown Scottsdale. The Downtown development standards include setback and stepback requirements to gradually pull the building away from the surrounding streets, reduce the overall apparent massing of buildings, and reduce shadows on surrounding properties. The development standards and design guidelines also encourage active street frontages and pedestrian areas by promoting pedestrian scale activities and treatments, such as and building openings at street level, landscaping and patios, and covered walks.

The Downtown PBD District amendment procedure allows development flexibility for land assembly, shared parking facilities, floor area, and height adjustments to develop a strong mixed-use character. Instead of complying strictly to the Downtown development standards with regular stepbacks, the applicant is proposing an alternative design utilizing more design flexibility while achieving the same downtown design goals. The following is a list of development standards that the applicant is requesting to amend:

| Required Standard | Amended Standard |
|--|--|
| 40 foot setback on Camelback | 10 foot avg. at first story 80 foot avg. at taller building areas |
| 20 foot setback on Brown | 10 foot average |
| 20 foot setback on Buckboard | 15 foot average |
| 20 foot setback on Shoeman | 10 foot average |
| 350 foot building size maximum on any one side | 390 foot building size maximum on any one side (one story podium) |
| 550 foot building size maximum on two adjacent sides | No change |
| 200 foot building size maximum above 38 foot height; 200 foot horizontal wall without architectural break | 275 foot building size maximum above 38 foot height on Buckboard; Architectural break redefined as changes in wall plane and façade treatment (bays and materials) |
| 2:1 stepback above 38 foot height (15 foot encroachment not to exceed 25% of building elevation) | Delete standard; replace with alternative design for setbacks and stepbacks |
| 25% of front bldg face below 26 feet at front building setback; 25% of first level width of projected street elevation must be at least 10 feet behind setback | 25% of bldg face setback 10 feet from one story podium line along Camelback |

The applicant's Amended Development Standards Summary (Attachment #1A) describes in detail the standards that the applicant is seeking to amend.

IMPACT ANALYSIS

Rezoning/Planned Block Development (PBD) Designation:

This rezoning proposal conforms to the Downtown Land Use Plan and supports the City's goals for mixed-use development in the Downtown. The Downtown section of the zoning ordinance allows larger Downtown areas, in this case a minimum of 100,000 sq. ft., to apply for the PBD designation. While the PBD allows applicants some measure of flexibility in site planning, it also requires that the owners of the site contribute 1% of the building valuation for public art.

The Zoning Ordinance (Section 5.3082. F.) contains criteria for the adoption of a PBD District. The criteria are listed below.

1. *That the development plan is consistent with the adopted Downtown Plan and other applicable policies, and that it is compatible with development in the area it will directly affect.*

This proposal is consistent with the Downtown Plan, which designates this area for a mix of uses, including hotel and residential uses. The proposed hotel/residences are programmed to compliment the surrounding entertainment uses and will not be burdened by the nearby nightlife activities.

2. *That the development plan contributes additionally, beyond the underlying regulations that otherwise apply, to the urban design objectives articulated for Downtown, and the deviations from the regulations that otherwise would apply are justified by compensating benefits of the development plan.*

One of major urban design goals in Downtown is to create a compact Downtown with an intensified and diverse mix of activities. This project helps to achieve that goal by maximizing the development potential on the site, by offering a variety of land uses that will compliment the existing downtown uses, and by creating active street frontages. The project at this location also helps serve as a catalyst for promoting downtown, creating synergies in downtown, and providing potential to strengthen the inter-connectivity of various blocks in the downtown.

3. *That the development plan includes adequate provisions for utilities, services, and emergency vehicle access, and, if warranted, connections between underground parking facilities.*

The proposed plan will provide adequate services, utilities and emergency vehicle access. The proposal also provides underground parking and sidewalk improvements.

4. *The projected traffic generated by the development plan will not exceed the capacity of affected streets.*

The traffic study shows that the proposed mixed-use development

will not create traffic that will exceed the capacities of affected streets, and streets will operate at acceptable levels of service.

5. *That the development plan will not significantly increase solar shading of adjacent land in comparison with development under prevailing conditions.*

The size, location, and configuration of the proposed building on the site have been modeled to show no significant increase of solar shading on adjacent properties. The larger massing of the building is significantly set back from Camelback Road.

Traffic.

Site access will primarily be provided by a proposed driveway on Camelback Road between Buckboard Trail and Brown Avenue. Secondary site access for the hotel and commercial uses is proposed on Buckboard Trail. Access for the condominium units is proposed to be separated from the hotel entrance, and will be located either Brown Avenue or Shoeman Lane. All on-site parking is proposed to be located in an underground parking structure. There are some public on-street parking spaces located on Buckboard Trail, Brown Avenue, and Shoeman Lane.

Under current zoning, development can occur that would generate similar traffic than what the proposed development will generate. The benefit of the proposed mixture of hotel, residential, restaurant, and retail land uses is that it will reduce the amount of site-generated traffic due to the natural interaction of these land uses. The site location will also encourage the use of alternative modes of transportation for those traveling to the site and for those residents who live on the site.

The approval of the proposed development plan will generate an estimated 1,998 trips per day, with approximately 116 trips generated in the a.m. peak hour and 189 trips generated in the p.m. peak hour. This represents an increase of approximately 1,000 daily trips when compared to the estimated traffic that would be generated if the site were developed as a mixture of restaurant and office land uses under the existing zoning. Adding retail development under the current zoning scenario would likely increase traffic potential for comparison purposes. (See Comparison Trip Generation Table below, and Attachment 8: Traffic Impact Analysis Summary)

Comparison Trip Generation

| Land Use | Daily Total |
|--|--------------------|
| Current Zoning (offices and restaurant) | 956 |
| Proposed Development Plan | 1,998 |
| Change | +1,042 |

Site traffic will be distributed primarily to Camelback Road via the unsignalized intersections of Buckboard Trail, Brown Avenue, and the proposed site driveway. Camelback Road is under capacity for current and projected traffic volumes. Capacity analyses for the Year 2010 indicate that

the most movements at unsignalized intersections along Camelback Road in the vicinity of the site will operate at level of service C or better. As is typical for unsignalized intersections on arterial streets, the left-turn movements onto Camelback Road operate at poor level of service at all of these intersections. The surrounding street network does provide alternative routes for drivers desiring to head westbound on Camelback Road. Traffic will not exceed the capacities of affected streets, and streets will operate at acceptable levels of service.

Alley Abandonment.

With the proposed development occupying the entire block, there is no need to maintain the existing alley right-of-way. However, until this site develops, emergency access and public utility easements will be reserved over the abandoned right-of-way. Letters of support from the affected public utility companies are on file with the City of Scottsdale subject to a public utility easement reservation for utilities. At the time of development, the applicant will need to work with affected parties to release and relocate all necessary easements.

Amended Development Standards.

While deviating from the strict standards of the Downtown District, the applicant proposes that the development will meet the same goals and objectives promoted by the standards. The amended standards are proposed to allow the applicant to construct a strong mixed-use project on a relatively small lot in downtown. The development proposes active street frontages by providing storefronts along the streets, pedestrian-scaled canopies, and courtyard and patio opportunities. The building massing is proposed to be designed with a variety of massing, architectural details, and building materials to reduce the apparent size of bulk of the building.

On November 4, 2004, the Development Review Board voted unanimously to recommend approval of the proposed amended development standards. The Board's review and recommendation is required per the procedural processing requirements within the Zoning Ordinance.

Floor Area.

The applicant proposes to maximize the site development to achieve the mixed-use development on this relatively small site. This includes a request to maximize the allowable floor area ratio in the Downtown District. The Downtown District allows bonus floor areas for developments that provide hotel/residential uses, underground parking, and that develop with the Planned Block Development designation. Further, the applicant proposes to provide special public improvements for right-of-way dedications and street/sidewalk enhancements to achieve the maximum floor area ratio bonus. These enhancements include pedestrian-scaled canopies, landscaping, courtyards and plazas, decorative paving and lampposts, street furniture, and other decorative finishes.

Downtown Group.

The Downtown Group has evaluated this proposal and notes the re-emergence

of "Luxury" rated hotel and resort properties in the downtown. Hotel/resort properties in this "luxury" category provide an important economic impact to the city through sales and bed taxes. With the addition of the proposed hotel, there would be an unprecedented three downtown hotels anticipated to carry the luxury designation (including the James Hotel and Valley Ho). These properties will play an important role in contributing to the revitalization efforts in downtown. The increased economic activity generated by these hotels contributes directly to the 1998 Downtown Plan Goal of "Promoting the downtown area as a prime residential/hotel center". This proposal also supports the goal in the 1998 plan of "insuring the economic viability of downtown."

Economic Vitality.

The proposed use is reasonable and appropriate within the downtown area and helps to maintain the competitiveness of this area in comparison to alternative available development sites situated in surrounding communities. This lodging product concept does not currently exist in the Scottsdale market area and will therefore be a unique product that will complement the existing hotels.

Parking.

Parking will be provided underground, and there is potential for parking improvements to the on-street parking on Brown Avenue and Buckboard Trail adjacent to this site.

Water/Sewer.

The developer will be required to extend and improve existing services in the surrounding streets to adequately serve the proposed development.

Police/Fire.

The proposal represents infill development in the downtown area and is not expected to impact police and fire services.

Schools District comments/review.

The Scottsdale Unified School District has been notified of this proposal and there are adequate school facilities to accommodate any students generated by the proposed project.

Policy Implications.

This proposal supports the City's goals for mixed-use development in the Downtown, provides for unique tourist accommodations in the downtown, and compliments the surrounding entertainment uses. The proposed building's height and architectural style will maximize the development potential on a relatively small property, while providing active street frontages and architectural details promoted by the Downtown Design and Architectural Guidelines.

Community Involvement.

Surrounding property owners have been notified, the site has been posted, and the applicant held two neighborhood meetings to discuss the proposal. The

Development Review Board supports the project after reviewing the architect's design approach and the proposed amended development standards. Staff has received general inquires and supportive comments regarding this proposal.

Community Impact.

The proposal provides a mixed-use development on a vacant underutilized block in the downtown. Traffic generated by the proposal is similar to what currently can be developed on the site and will not exceed the capacities of affected streets. The developer will be required to extend water and utility services to the site, and other services will not be impacted.

STAFF
RECOMMENDATION

Recommended Approach:

Staff recommends approval, subject to the attached stipulations.

OTHER BOARDS AND
COMMISSIONS

Development Review Board.

On November 4, 2004, the Development Review Board voted unanimously to recommend approval of the proposed amended development standards. The Board's review and recommendation is required per the procedural processing requirements within the Zoning Ordinance. There were no citizen comments made during the Development Review Board hearing, however the Board made the following comments:

- The development with the amended standards will meet the same goals and objectives promoted by the standards,
- Proposal demonstrates a strong pedestrian environment,
- The visual and pedestrian openings along Camelback Road into the porte-cochere area need to be strengthened,
- Restaurant location and orientation provides good pedestrian relationship,
- Special attention needs to be given toward Camelback elevation and building corners.

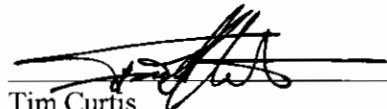
RESPONSIBLE
DEPT(S)

Planning and Development Services Department
Current Planning Services

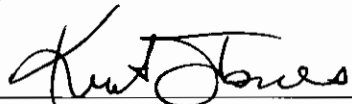
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APPROVED BY



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ATTACHMENTS

1. Applicant's Narrative
- 1A. Applicant's Amended Development Standards Summary
2. Context Aerial
- 2A. Aerial Close-Up
3. Land Use Map
4. Zoning Map
5. Abandonment Location Maps
6. Stipulations
7. Additional Information
8. Traffic Impact Summary
9. Citizen Involvement
10. City Notification Map
11. Site Plan

Project Narrative

W Scottsdale

Triyar Hospitality Scottsdale LLC

October 8, 2004

Development Team

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Project Introduction

Downtown Scottsdale has been the focus of much discussion, study and debate over the last several years regarding the apparent decline of its business and retail core. As a means to infuse the area with new life and secure the long term sustainability of the area, the City, business owners and citizens have expended significant time and energy to the subject of revitalizing Downtown. As a result, several important new projects, both public and private, around the perimeter of Downtown are currently underway, but Downtown Scottsdale still needs to embrace a broader range of uses to secure its future. The W Scottsdale Hotel and Residences has the potential to be a very important cornerstone in the future of Downtown Scottsdale.

The project site, which is approximately two (2) acres located on the south side of Camelback Road between Brown Avenue and Buckboard Trail, must be rezoned from C-2, C-3, P-2 and P-3 DO to Downtown Office/Residential Type 2 Planned Block Development Downtown Overlay (D O/R Type 2 PBD DO) with Amended Development Standards. An Abandonment of an existing alley on site is also part of this application.

The W Hotel

Starwood Hotels and Resorts (parent company of W Hotels) in collaboration with Triyar Hospitality Scottsdale L.L.C. is proposing to bring the W Scottsdale Hotel and Residences into the heart of what has become known as Scottsdale's Entertainment District. The W concept for Scottsdale includes a traditional hotel component, a spa, specialty retail combined with a full service whole ownership condominium element. Scottsdale is one of a few select cities that have been chosen for development of the new W Hotel and Residence concept. Dallas, Texas, South Beach, Florida and Ft. Lauderdale, Florida also have similar projects under development. Additionally, the W San Diego has recently been completed in the downtown Gaslamp entertainment district. This property has been extremely successful and has many parallels to the Scottsdale location.

W Hotels are known for their world-class design and energetic ambiance. The W Hotel experience is unique within the travel industry and the W is viewed as a destination address. The W Scottsdale is anticipated to be an anchor for Downtown Scottsdale's Entertainment District and will serve as a catalyst in the ongoing revitalization of the Downtown area. Scottsdale has a growing population of young professionals, an important component of the "knowledge workers," who are an vital element in spurring the local economy and contribute greatly to economic development. As a proven attraction for this demographic segment, the W Scottsdale will become a destination. Similar W projects in Dallas, Texas and Ft. Lauderdale and South Beach, Florida are being viewed by those cities as centerpieces in the revitalization and development of the areas in which they are located.

The W Hotel is a component of Starwood Hotels and Resorts Worldwide Inc. Starwood is one of the leading hotel and leisure companies in the world with more than 750 properties in more than 80 countries. With internationally renowned brands, Starwood is a fully integrated owner, operator and franchisor of hotels and resorts including St. Régis, The Luxury Collection, Sheraton and Westin.

Triyar Hospitality Scottsdale, L.L.C., is a local development company with an extensive resume of retail and residential projects in Arizona, Texas and California. Local development projects include:

Renaissance Specialty Retail Center. Located at the NEC of Scottsdale and Camelback Roads in the heart of downtown Scottsdale, this 40,000 square foot retail center was completed in 2002.

24th Street and Baseline Retail Center & Residential Development. Located at the southeast and southwest corners of 24th Street and Baseline Roads in Phoenix, this 70 acre mixed-use development includes 25 acres of regional commercial and 47 acres of residential development.

Copper Falls, a master planned residential community in Buckeye. Located at the NWC of Southern and Miller Roads, this two-phased, 280 acre community includes six distinctive residential neighborhoods, two commercial centers and an office / medical park.

Existing Site Conditions

The project site is comprised an approximately two (2) acre city block, currently vacant and zoned C-2, C-3, P-2 and P-3 DO. The site is located on the south side of Camelback Road between Buckboard Trail and Brown Avenue. The site is located within the Downtown Overlay district.

Importantly, this site is located in the heart of Downtown Scottsdale's emerging Entertainment District with frontage on Camelback Road. The immediate area has a strong nightlife component that coexists with daytime office uses to create a very vibrant setting for the hotel. Tremendous opportunities for shopping exist as the site is close to the gallery district and Scottsdale Fashion Square. There are a number of restaurants in the immediate area as well as civic destinations such as the Scottsdale Municipal Stadium, Scottsdale Center for the Arts and Scottsdale Museum of Modern Art. The currently under construction Scottsdale Waterfront project will also create additional draw for Downtown Scottsdale.

Rezoning Request and Detailed Project Description

Downtown Scottsdale is currently experiencing a revival with public and private investment exceeding \$559 million and the development of several important projects including the Scottsdale Waterfront mixed-use development, Hotel Valley Ho renovation and The James Hotel. Several other residential, retail and office projects are currently undergoing review by the City that will further enliven Downtown if approved. The W Scottsdale will be a very important component in the continuing successful effort to revitalize Downtown Scottsdale. The W Scottsdale Hotel + Residences proposal has the potential to create a very synergistic relationship with other existing uses in the area. The hotel and its guests are a natural compliment to the high energy entertainment uses located in this area of Downtown Scottsdale. The hotel may also be able to establish a very beneficial relationship with the Galleria office building and other office facilities in the area by providing an amenity rich, convenient destination for business travelers.

Specifically, the W Scottsdale proposal is a mixed-use hotel, spa and residential property. This seven (7) story (not to exceed 72 feet in height pursuant to the City of Scottsdale Zoning Ordinance) 225 room high-end hotel will also include 25 private residences, a signature restaurant, ballroom and several private meeting rooms as well as street front specialty retail space. Primary parking for the project will be provided by an on-site underground parking garage.

The building itself, as a Scottsdale style "urban" hotel, is intended to address both the neighborhood and specific block on which it sits. The project's north facing Camelback frontage is recognized as giving identity on a greater scale, while its other frontages provide significant place-making and pedestrian opportunities for Scottsdale's downtown on a smaller, neighborhood scale.

The project's massing and position on the site are governed substantially by the hotel guestroom program and the Zoning Ordinance height limitation. The hotel and residences have distinct wings that spread along the Brown Avenue and Buckboard Trail street fronts and is situated above a lower podium element housing the hotel's public and retail areas. The guestroom and residential floors are a narrow 49 feet in width, in a double loaded configuration with over 50 rooms per floor, providing guests and residents with expansive views wherever possible. Below these, the hotel's public function faces out to the neighborhood in an urban relationship, creating a street edge along most of the site. On site parking will be below grade, accessed directly from the hotel entry for convenient valet service for guests.

Active public functions in the building will be directly accessible from the street, with the signature restaurant anchoring the important corner of Camelback Road and Brown Avenue, allowing visitors to experience the building and the vibrancy of the hotel approaching from the west. The project's "front door" will be a porte cochere entry facing Camelback Road, with a deceleration lane on site to bring cars safely inside. The Camelback frontage includes a one story pool, spa and fitness deck above the entry, keeping the higher mass of the building set back significantly from the property line.

Along Buckboard Trail, a series of one story stone clad storefront openings on a widened sidewalk (approximately 15 feet) include additional streetscape functions, as well as another public entry for the hotel. The hotel's namesake "W" boutique, along with other retail lease spaces will share the Buckboard Trail façade with a small residential entry and garden, and a storefront entry for the spa. At the corner of Buckboard Trail and Shoeman Lane, there will be a shaded outdoor function terrace. This arrangement is intended to bring activity and a pedestrian friendly scale to the Buckboard Trail side, and create an interactive dialogue with the Entertainment District concept for the neighborhood as a whole. A series of protruding canopies will provide both shade and reduced scale at the sidewalk, with street trees to compliment those found on the west side of the street.

Along the project's Shoeman Lane and Brown Avenue frontages, the single story podium houses a ballroom and meeting area, and back of house hotel functions such as employee areas, administration and kitchen, with the guestroom and residential tower set back from the street above the podium level. The materials envisioned for these pedestrian level areas include stone and clear glazing at the retail storefronts, with significant landscaping at the entry and in planters at key points around the building.

Within the hotel, the center of activity will be the signature "living room" lobby area, for which W hotels are well known. The living room features lounge and seating areas, a bar, and is flanked by both the restaurant and hotel front desk functions. The ballroom and meeting areas are accessed through a gallery space, which also brings guests to the hotel room elevators. A grand stair will draw guests up from the living room to the pool deck above. The pool deck program includes an indoor/outdoor meeting area, fitness and spa facilities, and dining and bar areas. The pool itself will be flanked by extensive lounge areas and pool cabanas, positioned to the to capture winter sun and views of Camelback Mountain. Closer to the tower, a more secluded series of suites with private terraces and outdoor spa area have direct access to the pool.

The hotel guestrooms will continue from level two up through level portions of level six, oriented both towards the pool deck below and outward to views of the mountains and Downtown Scottsdale. Guests will arrive via a bank of high-speed elevators located on the Buckboard Trail side of the hotel, and circulate via corridor to rooms. Larger hotel suites are positioned at the northeast and northwest corners facing Camelback Road and the mountains to afford increased views and plan flexibility. Rooms in these locations will focus around sliding glass walls that open towards Camelback Road, adding depth to the façade and bringing vibrant hotel activity out into areas visible from the street. The materials for the guestroom exterior will include high performance glazing in aluminum framing, set against a warm toned background material to compliment the stone below.

Private residences will begin on level six, with level seven being dedicated entirely to residential. These levels have exclusive elevator service, allowing direct access from dedicated parking stalls up to a private pedestrian entry and hotel public areas, including the pool deck. While the exterior of the residences will closely resemble the hotel areas, additional balconies will punctuate the façade at key points. Set back from the façade at the top of the building, a penthouse will occupy a portion of the roof to house common mechanical equipment and elevator machinery.

Amended Development Standards

The following is a description of the proposed Amended Development Standards for the W Scottsdale:

| Ordinance Section | Type of Standard | Proposed Development Standard |
|------------------------------|-------------------------------------|--|
| 5.3060.I and 5.3090 | Floor Area Ratio | 1.8 FAR to include: <ul style="list-style-type: none"> .8 ordinance allowed FAR .3 underground parking bonus .1 PBD bonus .4 Residential/Hotel bonus .2 Special Incentive bonus |
| 5.3061.H | Minimum Front Building Setback | Camelback Rd. - average 10 feet Shoeman Lane - average 10 feet Buckboard Trail - average 10 feet Brown Avenue - average 10 feet |
| | Parking | TBD pending results of a parking analysis |
| 5.3061.D | Building Size Maximum | Brown Avenue – 400 feet below 38 feet Buckboard Trail – 400 feet below 38 feet Brown Avenue – 257 feet above 38 feet Buckboard Trail – 264 feet above 38 feet Front and Side – 586 feet |
| 5.3060 III 5.b 5.3061.F.1 | Large Walls – Horizontal Dimension | Buckboard Trail – 264 feet Brown Avenue - 257 |
| 5.3060 III 5.a 5.3061.F.2 | Large Walls – Vertical Dimension | Waive requirement for limits of the “vertical dimension” of large walls. |
| 5.3060 III.6 | Building Envelope | Waive the required building envelope requirements for all locations on the site where the encroachments beyond the building stepback plane occur. |
| 5.3060 III.7 | Encroachments Beyond Stepback Plane | Allow the vertical encroachment to exceed 15 feet on a maximum of 100% of the length of the building on all street frontages. |
| 5.3060 III.8 | Building Lines | Waive the requirement that a minimum of 25% of width of projected street elevation must be at least 10 feet behind front building setback on all street frontages. |

*** discussion and justification of specific amended development standards to be submitted at a later date**

Cultural Improvements Program

****to be submitted at a later date***

Abandonment Application

There is an existing alley located on the site, which must be abandoned to facilitate development of the hotel. The alley does not serve any existing development, and accordingly there is no outside impact associated with the abandonment request.

**Triyar Hospitality Scottsdale, LLC
W Hotel Scottsdale**

Special Improvements Proposed Program

The intent of Triyar's special improvement program is to develop the streetscape surrounding the W Hotel to a level that is commensurate with the new image of the Scottsdale Entertainment District (yet to be defined by the City and Triyar).

Triyar Hospitality will work with the City as it moves forward with its plans to be sure that the design of the special improvements program is compatible with the City's needs.

The improvements proposed are as follows:

- Sidewalks surrounding the W Hotel will be in a pattered stone finish.
- The sidewalk on Buckboard will be widened to 15 feet so as to create a pedestrian/retail friendly experience. The balance of the sidewalks around the W Hotel will be approximately 10 feet wide.
- Approximately 30 specially design lamp posts will be installed around the W Hotel and on the east side of Buckboard.
- Approximately 30 mature trees will be planted in the sidewalks around the W Hotel and on the east side of Buckboard. These trees will be surrounded by oversized ornamental grading.
- Approximately 10 sidewalk benches will further enhance the Buckboard Trail pedestrian/retail experience.

Preliminary estimates of the cost for the special improvements are between \$300,000.00 and \$400,000.00. Triyar is prepared to meet with the City to discuss these proposed special improvements in the near term.

Triyar Hospitality Scottsdale, LLC
W Hotel Scottsdale

Special Improvements Proposed Program

The intent of Triyar's special improvement program is to develop the streetscape surrounding the W Hotel to a level that is commensurate with the new image of the Scottsdale Entertainment District (yet to be defined by the City and Triyar).

Triyar Hospitality will work with the City as it moves forward with its plans to be sure that the design of the special improvements program is compatible with the City's needs.

The improvements proposed are as follows:

- Sidewalks surrounding the W Hotel will be in a pattered stone finish.
- The sidewalk on Buckboard will be widened to 15 feet so as to create a pedestrian/retail friendly experience. The balance of the sidewalks around the W Hotel will be approximately 10 feet wide.
- Approximately 30 specially design lamp posts will be installed around the W Hotel and on the east side of Buckboard.
- Approximately 30 mature trees will be planted in the sidewalks around the W Hotel and on the east side of Buckboard. These trees will be surrounded by oversized ornamental grading.
- Approximately 10 sidewalk benches will further enhance the Buckboard Trail pedestrian/retail experience.

Preliminary estimates of the cost for the special improvements are between \$300,000.00 and \$400,000.00. Triyar is prepared to meet with the City to discuss these proposed special improvements in the near term.

PROPOSED LAND USE PLAN

The current sit development plan includes the following mixture of land uses:

- Public Assembly Space – 6,730 square feet, which includes the ballroom of 3,500 square feet, and three meeting rooms of 1,290 square feet, 1,100 square feet, and 840 square feet.
- Restaurant – 4,050 square feet, comprised of 2,800 square feet of public dining area served by a 1,250 square foot kitchen.
- Bar – 4,500 square feet separated into two lounges, one consisting of 2,300 square feet of public area supported by 600 square feet of back-to-bar area. The second consisting of 1,100 square feet supported by 500 square feet of back-of-bar area
- Retail Space – 4,450 square feet

W Scottsdale Hotel and Residences
Amended Development Standards Summary
22-ZN-2004 & 19-AB-2004
Submitted October 20, 2004

The following is a list of Zoning Ordinance Development standards for which amendments are proposed as part of a rezone to the D Downtown District.

General notes:

While the project setbacks are determined from the planned curb line, the site property line is outside the proposed curb line for the majority of the site frontage. In general, the project's mass is composed of a guestroom and residential element of six stories which sits above a one story podium element. The taller portion is set back from the podium on three sides, and its size and bulk are broken up into a number of "bay window" volumes on each street frontage to suggest a series of smaller, residential scale buildings. For continuity of street spaces, each street frontage has a one story podium or "base" expression which is parallel to the street, reducing gaps of undefined space.

At the street level, an additional emphasis has been placed on concentrating public functions (such as retail, spa etc.), with frequent openings, along the east (Buckboard) side of the site in to reinforce current and future pedestrian activity. The remaining three sides have additional public functions, including restaurant, hotel entry, function terrace, employee training and break areas, etc. These street level elevations will utilize a combination of rich materials such as stone cladding, along with scale defining elements such as canopies and expressed columns, to create pedestrian interest. Blank, flat walls without openings will be eliminated, and utilize glazed display openings where windows are not feasible. On each frontage, minimum sidewalk width of 10' will allow street trees and additional landscaping.

Proposed Amendments:

5.3061H Camelback Road setback of 40'.

Proposed Amendment: Single story average 10' setback, taller building areas average 80' setback.

The portion of the project which reaches maximum height is set back significantly from the Camelback frontage, with a minimum of 80'. In order to create an active street front, the hotel porte cochere and restaurant are set back between 30' and 20' at Brown and Camelback, with the single story porte cochere set back at least 10' from planned curb west of the entry, and at least 22' east of the entry. This helps make a definitive streetscape and brings a public function of the project out to this important corner. The proposed setback also allows for outdoor dining adjacent to the sidewalk.

5.3066 Minimum Front Building Setback. Minimum 20' front setbacks at Buckboard Trail, Shoeman Lane, and Brown Avenue.

Proposed Amendment: 15' average setback at Buckboard, 10' average at Shoeman and Brown.

The project's location creates a unique setback condition, with no side or rear orientations (which would carry no setback requirements). In order to encourage a more pedestrian friendly and active street frontage, the taller portions of the building are set back from the sidewalk on the majority of its' north, south, and west facades. These single story elevations greatly reduce the scale at the sidewalk level and have average 10' setbacks from planned curb, with some areas set back 20'.

On the east (Buckboard) side, the taller facade is set back 15' average, creating a wider sidewalk in front of the storefront areas on this side. This extra width will allow upgraded amenities such as benches, street trees, and streetlamps along this edge. To further mitigate this reduced setback, a series of protruding canopies cover the sidewalk, provide shade, help define the "base" of the façade, and make significantly smaller scale for pedestrians.

5.0361D,F Building Maximum Size. Maximum 350' building length along one side, 550' along two adjacent sides. 200' maximum building length maximum above 38'. 200' Maximum without "break".

Proposed Amendment: Maximum 390' of one story podium along one side. Maximum 275' above 38' with significant changes in plane and façade treatment for walls more than 200'.

The single story podium which forms the project's streetscape extends along the majority of the site, exceeding the 350' maximum at 390' on the west side. To mitigate this length, the design will include significant changes in both material and vocabulary on all sides, maximizing interest and reduced scale at the sidewalk. Along the Buckboard side, this will include a colonnade parallel to the street. In addition, frequent building entrances, especially along Buckboard, allow access to active public and non-public functions which have sidewalk exposure, including back-of-house functions such as training and employee break areas. A shaded outdoor dining area is envisioned for the Camelback and Brown corner. The Function Terrace at Shoeman and Buckboard is an indoor/outdoor space visible from the sidewalk and street, shaded and screened with landscaping and fabric canopies.

The proposed project includes 225 hotel guestrooms, configured in a double loaded corridor arrangement. In order to stay below the maximum building height, the taller guestroom areas are arranged in distinct "wings" which measure 255' on the west side of the site and 275' on the east side. These exceed the 200' maximum length above 38'. In order to mitigate these longer walls, a combination of change in plane and change in materials are used to break them down in scale. Along the west (Brown) façade, the entire wall is rotated 9 degrees in plan after the initial 160', with a large notch at the inside corner. This allows the remaining 95' to set back an additional 10'. Along the east (Buckboard) façade the wall also has an inset notch after the initial 200', and the remaining 75' is designed with a significantly different vocabulary, utilizing deeply inset punched openings in a contrasting material type and color.

5.0361 J, N Building Envelope. 2:1 step back above 38' height.

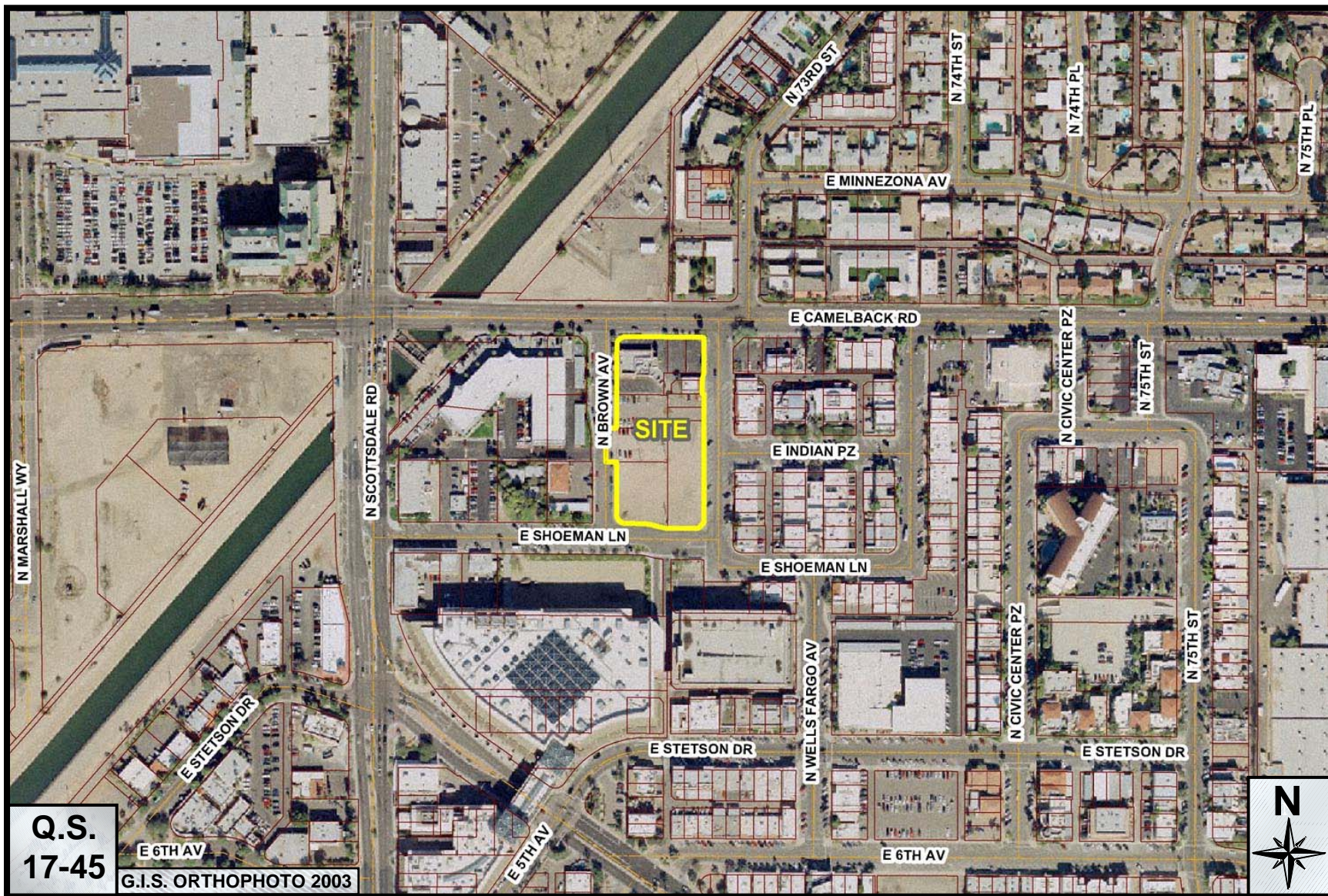
Proposed Amendment: Significant step backs above 17.5' podium wall on north, west, and south facades. Additional step back at topmost occupied level.

While the majority of the building's taller facades step back from the street and podium level, the walls above 38' do not step back at 2:1. The project's hotel guestrooms begin on the second level, continuing for a total of five levels. Each guestroom has a bathroom, each oriented back-to-back and stacked above the bathroom below. This arrangement is necessary to allow for the intensive plumbing associated with more than 225 bathrooms. Stepping the guestrooms back would offset this plumbing, and require the hotel guestrooms to become smaller on the building's upper floors. With the maximum building height, the hotel's guestrooms cannot reasonably fit if stepped back. The highest floor in the building will be residences, which can be stepped back from the guestrooms below.

5.3063 Building Lines. a. Minimum 25% of building face at front building setback. b. At first level, minimum 25% width of projected elevation must be at least 10' behind front building setback.

Proposed Amendment: 25% of building face set back 10' from one story podium line along Camelback.

a. (see 5.3061H above) b. While the projected elevation of the restaurant is not 10' behind the front building setback line, it is set back at least 10' from the remainder of the front façade, and is at least 25% of that frontage.



W Hotel Scottsdale

22-ZN-2004

ATTACHMENT #2



Q.S.
17-45

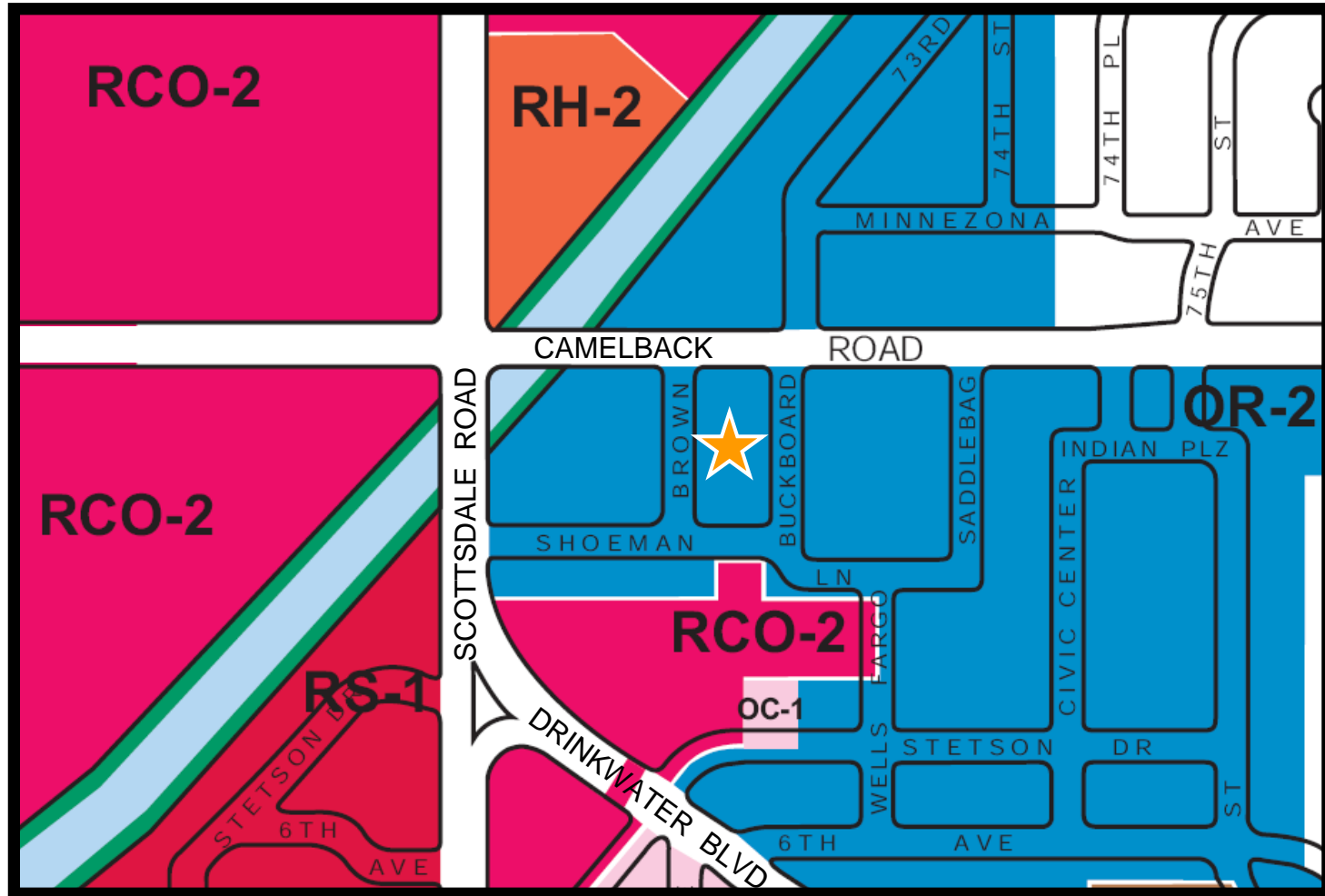
G.I.S. ORTHOPHOTO 2003









W Hotel Scottsdale

22-ZN-2004

ATTACHMENT #2A

Downtown General Plan

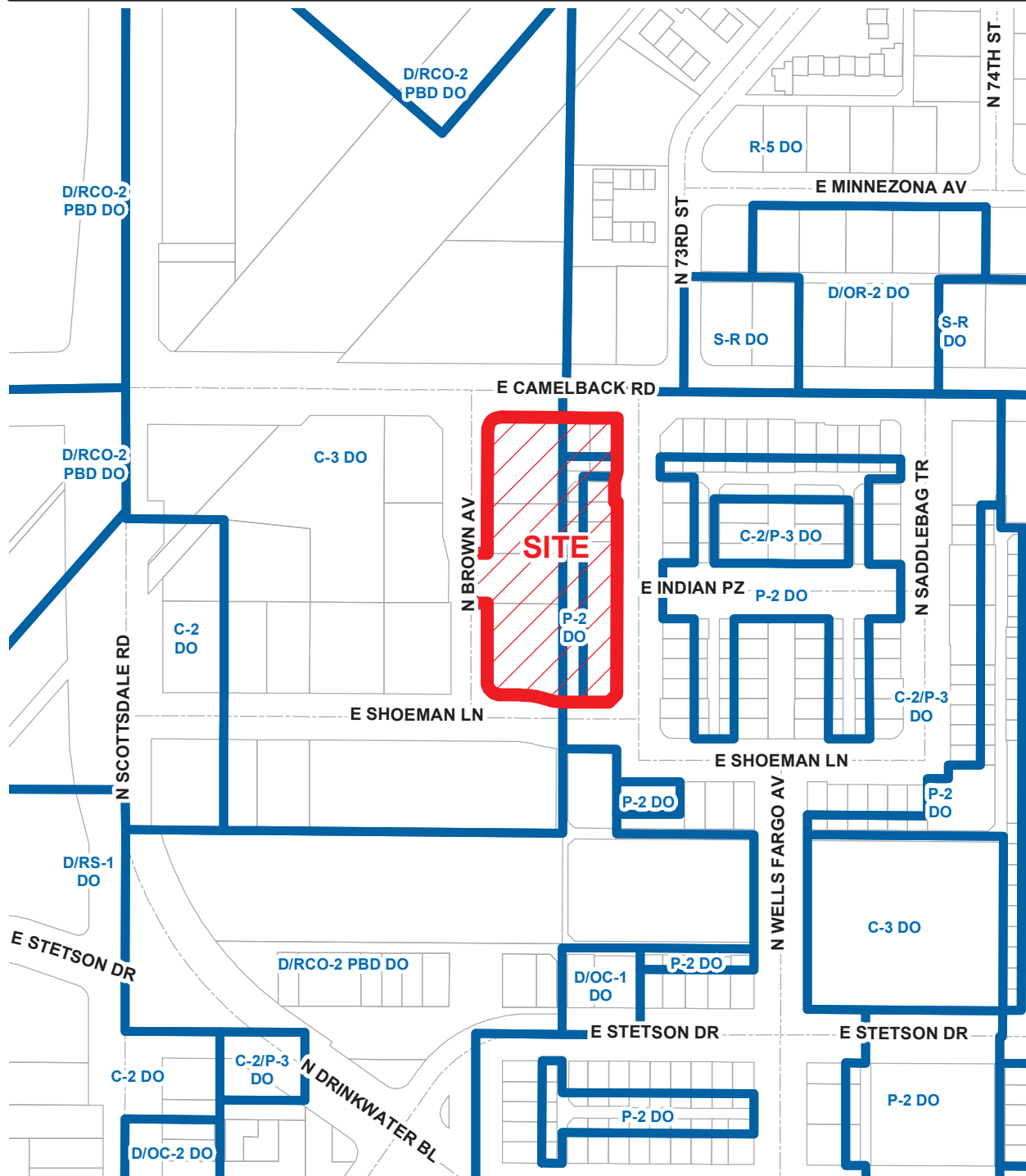


| | | | |
|---|--------------------|---|----------------------------|
|  | Retail Specialty |  | Residential/Hotel |
|  | Office Commercial |  | Regional Commercial/Office |
|  | Civic Center |  | Residential High Density |
|  | Office Residential |  | Medical |



22-ZN-2004
ATTACHMENT #3

Rezone from Central Business District, Highway Commercial District, Parking Districts and Downtown Overlay to rezone the property to Downtown/Office Residential District Type 2, Planned Block Development, Downtown Overlay (D/OR-2 PBD DO) with amended development standards



22-ZN-2004

ATTACHMENT #4



Q.S.
17-45

G.I.S. ORTHOPHOTO 2003

W Hotel Scottsdale

19-AB-2004

ATTACHMENT #5

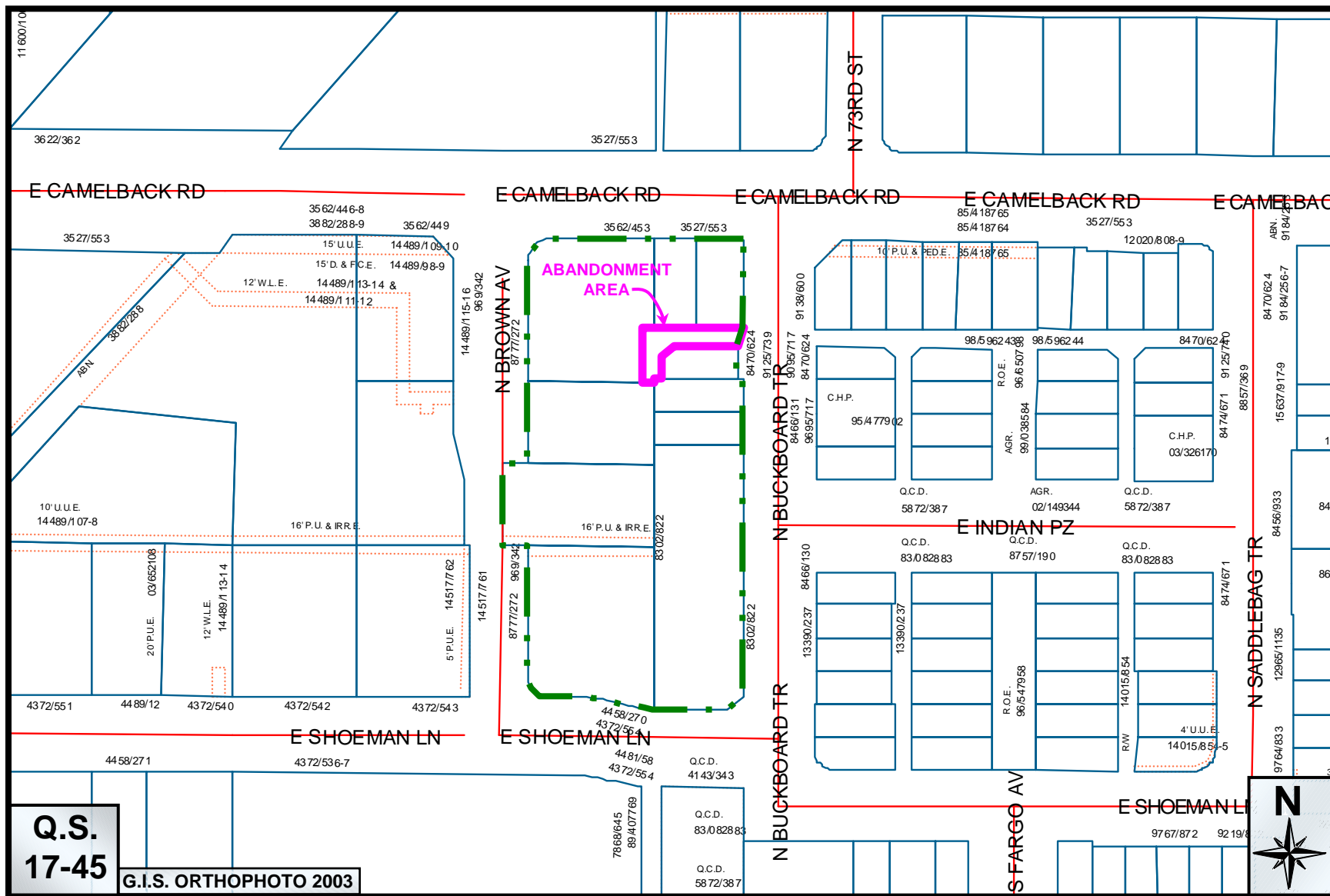


Q.S.
17-45

G.I.S. ORTHOPHOTO 2003

W Hotel Scottsdale

19-AB-2004



W Hotel Scottsdale

19-AB-2004

Easements & Right-of-way

STIPULATIONS FOR CASE 22-ZN-2004 AND 19-AB-2004

PLANNING/ DEVELOPMENT

1. CONFORMANCE TO THE SITE PLAN. Development shall conform with the site plan submitted by Hornberger and Worstell Architects & Planners and dated 10/19/2004. These stipulations take precedence over the above-referenced site plan. Any proposed significant change, as determined by the Zoning Administrator, shall be subject to subsequent public hearings before the Planning Commission and City Council.
2. FLOOR AREA RATIO (FAR) – BONUSES. Except as otherwise specified herein, and subject to the establishment and maintenance of qualifying facilities and uses, the subject site is granted floor area ratio bonuses for the following:
 - A. Underground parking (not to exceed 0.3 floor area ratio bonus),
 - B. Special improvements for right-of-way dedications and street/sidewalk improvements (not to exceed 0.3 floor area ratio bonus)
 - C. Planned Block Development (0.1 floor area ratio bonus)
 - D. Residential and hotel (0.4 floor area ratio)
 - E. The maximum total floor area ratio for the site shall not exceed 1.8.
3. APPLICABILITY OF DEVELOPMENT STANDARDS. Except for the development standards specifically modified herein, and on the Amended Development Standards summary dated 10/20/2004 and attached, all improvements on the subject site shall comply with the development standards of the Scottsdale Zoning Ordinance.
 - A. SETBACK AMENDMENTS. On the Camelback Road frontage, an average ten (10) foot setback shall be provided for the first story portion, and an average eighty (80) foot setback for the taller building portions on the north side of the building. On the Buckboard Trail frontage, an average fifteen (15) foot setback shall be provided. On the Shoeman Lane frontage, an average ten (10) foot setback shall be provided, and on the Brown frontage, an average ten (10) foot setback shall be provided. All setbacks shall be measured planned curb.
 - B. BUILDING SIZE MAXIMUM ON ANY ON SIDE OF THE BUILDING. The maximum length of a building on any one side shall not exceed 390 feet.
 - C. BUILDING SIZE MAXIMUM LENGTH. The maximum length of a building side above thirty eight (38) feet in height shall not exceed 275 feet.
 - D. BUILDING ENVELOPE. The proposed building massing design for setbacks and stepbacks shown in site plan and 3-D massing model views submitted by Hornberger and Worstell Architects & Planners and dated 10/19/2004 shall replace the inclined stepback development standards of the (D) Downtown zoning district.
 - E. BUILDING LINES. Twenty-five (25) percent of the building face shall be setback ten (10) feet from the one story podium line along the Camelback Road frontage.
4. PEDESTRIAN MASTER PLAN. With Development Review Board submittal, the developer shall submit a Pedestrian Master Plan showing pedestrian openings and connections, sidewalk widths and improvements, pedestrian-scaled canopies, landscaping, courtyards and plazas, decorative paving and lampposts, street furniture, and other decorative finishes to be provided by the developer.
5. PARKING. With Development Review Board submittal, the developer shall submit a parking analysis and/or parking master plan demonstrating that parking will be provided with this

development to accommodate the generated demand, in compliance with the City's Zoning Ordinance.

6. ENTERTAINMENT AREA DISCLOSURE. With final plans submittal, and in a form acceptable to final plan review staff, the developer shall submit documentation recorded with the property that discloses the following: *This property is located in an active downtown entertainment area and may be subject to ambient entertainment noise.*
7. UTILITY LINES. With this development, the developer shall be responsible for removing all existing above ground utility lines and poles on this block (behind the street curbs adjacent to the property), to be replaced with underground utility lines as necessary. This may involve redirecting utility lines under adjacent streets to continue service to nearby properties.
8. ALLEY ABANDONMENT. The property owner shall reserve a public utility easement over, under, and across the abandoned alley right-of-way, and shall reserve an emergency and service vehicle access easement over and across the abandoned alley right-of-way. At the time of development, the applicant shall work with affected parties to release and relocate all necessary easements.

CIRCULATION

1. STREET CONSTRUCTION. Before issuance of any certificate of occupancy for the site, the developer shall construct the following improvements, in conformance with the Design Standards and Policies Manual:
 - a. The developer shall construct a minimum 15-foot wide sidewalk/pedestrian area along Buckboard Trail and a minimum 10-foot wide sidewalk/pedestrian area along all other public roadways, or an approved alternate pedestrian route, as determined by city staff.
 - b. All public improvements shall be demonstrated on plans submitted to the Development Review Board.
2. STREET DEDICATIONS: Before issuance of any certificate of occupancy for the site, the owner/developer shall dedicate five (5) feet of additional public right-of-way (total 45 feet) along Camelback Road to the City. Before issuance of any certificate of occupancy for the site, the owner/developer shall also dedicate public right-of-way over all existing roadway easements on Buckboard Trail, Brown Avenue, and Shoeman Lane.
3. ACCESS RESTRICTIONS. Before issuance of any certificate of occupancy for the site, the developer shall dedicate the necessary right-of-way, as determined by city staff, and construct access to the site. Access to the site shall conform to the following restrictions (distances measured to the driveway or street centerlines unless otherwise noted):
 - a. Camelback Road – There shall be a maximum of one site driveway on Camelback Road. The driveway shall be located midway between Buckboard Trail and Brown Avenue. The developer shall dedicate a one-foot wide vehicular non-access easement on this street except at the approved site entrance.
 - b. Buckboard Trail – Site driveways shall be located a minimum distance of 50 feet from the Camelback Road right-of-way.
 - c. Brown Avenue - Site driveways shall be located a minimum distance of 50 feet from the Camelback Road right-of-way.
4. AUXILIARY LANE CONSTRUCTION. Before issuance of any certificate of occupancy for the site, the developer shall dedicate the necessary right-of-way, as determined by city staff, and construct a right-turn deceleration lane at the site entrance on Camelback Road, unless otherwise

directed by the Transportation General Manager.

5. RIGHT-OF-WAY ABANDONMENT. Abandonment of the existing on-site alley rights-of-way, as reflected in case 19-AB-2004, shall be completed prior to submittal of final improvement plans for the project.

ADDITIONAL INFORMATION FOR CASE 22-ZN-2004 AND 19-AB-2004

PLANNING/DEVELOPMENT

1. **DEVELOPMENT CONTINGENCIES.** The approved development program, including intensity, may be changed due to drainage issues and other site planning concerns which will need to be resolved at the time of site plan approval. Appropriate design solutions to these constraints may preclude achievement of the proposed development program.
2. **DEVELOPMENT REVIEW BOARD.** The City Council directs the Development Review Board's attention to:
 - a. the type, height, design, and intensity of buildings, site walls, and other structures,
 - b. visual and pedestrian openings along Camelback Road into the porte-cochere area,
 - c. pedestrian amenities, such as sidewalk improvements, pedestrian-scaled canopies, landscaping, courtyards and plazas, decorative paving and lampposts, street furniture, and other decorative finishes,
 - d. pedestrian connections,
 - e. streetscape improvements, including any modifications to on-street parking.

ENGINEERING

1. **RESPONSIBILITY FOR CONSTRUCTION OF INFRASTRUCTURE.** The developer shall be responsible for all improvements associated with the development or phase of the development and/or required for access or service to the development or phase of the development. Improvements shall include, but not be limited to washes, storm drains, drainage structures, water systems, sanitary sewer systems, curbs and gutters, paving, sidewalks, streetlights, street signs, and landscaping. The granting of zoning/use permit does not and shall not commit the city to provide any of these improvements.

DRAINAGE AND FLOOD CONTROL

1. **CONCEPTUAL DRAINAGE REPORT.** With the Development Review Board submittal, the developer shall submit a conceptual drainage report and plan subject to city staff approval. The conceptual report and plan shall conform to the Design Standards and Policies Manual - Drainage Report Preparation.

WATER

1. **BASIS OF DESIGN REPORT (WATER).** Before the improvement plan submittal to the Project Quality/Compliance Division, the developer shall submit a basis of design report and plan subject to Water Resources Department approval. The basis of design report shall conform to the Design Standards and Policies Manual.
2. **APPROVED BASIS OF DESIGN REPORT.** Before the improvement plan submittal to the Project Quality/Compliance Division, the developer shall have obtained approval of the Basis of Design Report.
3. **NEW WATER FACILITIES.** The developer shall provide all water lines and water related facilities necessary to serve the site.

WASTEWATER

1. BASIS OF DESIGN REPORT (SANITARY SEWER).). Before the improvement plan submittal to the Project Quality/Compliance Division, the developer shall submit a basis of design report and plan subject to Water Resources Department approval. The basis of design report shall be in conformance with the Design Standards and Policies Manual.
2. APPROVED BASIS OF DESIGN REPORT. Before the improvement plan submittal to the Project Quality/Compliance Division, the developer shall have obtained approval of the Basis of Design Report.
3. NEW WASTEWATER FACILITIES. The developer shall provide all sanitary sewer lines and wastewater related facilities necessary to serve the site.

OTHER REQUIREMENTS

1. DUST CONTROL PERMITS. Before commencing grading on sites 1/10 acre or larger, the developer shall have obtained a Dust Control Permit (earth moving equipment permit) from Maricopa County Division of Air Pollution Control. Call the county 602-507-6727 for fees and application information.
2. UTILITY CONFLICT COORDINATION. With the improvement plan submittal to the Project Quality/Compliance Division, the developer shall submit a signed No Conflict form (not required for city owned utilities) from every affected utility company.
3. CITY CONTROL OF ACCESS. The city retains the right to modify or void access within city right-of-way. The city's responsibility to promote safe conditions for the traveling public takes precedence over the stipulations above.

**W HOTEL
SWC CAMELBACK ROAD AND BUCKBOARD TRAIL
TRAFFIC IMPACT ANALYSIS SUMMARY**

**Summary Prepared by: Phillip Kercher, COS Traffic Engineering
Traffic Impact Study Prepared by: Dan Hartig, Parsons Brinckerhoff**

Existing Conditions:

The subject site is located along the south side of Camelback Road between Scottsdale Road and Miller Road. The site frontage on Camelback Road extends from Brown Avenue to Buckboard Trail, approximately 225 feet. The site extends south from Camelback Road to Shoeman Lane, approximately 440 feet.

East of Scottsdale Road, Camelback Road is designated as a Citywide System street on the Mobility Element of the City's General Plan. This section of Camelback Road is designated as a minor arterial on the City's Streets Master Plan. The street is improved to a five-lane cross section, two through lanes in each direction with a center two-way left-turn lane. The current daily volume of traffic using this section of Camelback Road is 20,500 vehicles. A five-lane cross street section without a raised median has a design capacity of 30,000 vehicles per day.

Scottsdale Road is designated as a Regional System Street on the Mobility Element of the City's General Plan. It is designated as a major arterial street on the City's Streets Master Plan. The street is improved to a full six-lane arterial cross section in the vicinity of Camelback Road. The current daily volume of traffic using this section of Scottsdale Road is 40,600 vehicles north of Camelback Road and 27,900 vehicles to the south.

Miller Road is designated as a Neighborhood Systems Street on the Mobility Element of the City's General Plan. On the City's Streets Master Plan, Miller Road is designated as a major collector street south of Camelback Road and a minor collector north of Camelback Road. The major collector section is improved with two southbound lanes and two northbound lanes separated by a raised median; the minor collector section has one lane each direction separated by a two-way left-turn lane. The current daily volume of traffic using this section of Miller Road is vehicles south of Camelback Road.

Marshall Way, Montecito Avenue, and Shoeman Lane are not designated on the Mobility Element of the City's General Plan or the Streets Master Plan. They are all designed to a local commercial cross section – two lanes in each direction with some on-street parking. The average daily traffic volumes on these streets are all less than 5,000 vehicles per day: Buckboard Trail has 2,700 daily vehicles; Brown Avenue has 1,300 daily vehicles; and Shoeman Lane has 2,400 daily vehicles.

All four major intersections around the site are currently unsignalized: Camelback Road and Buckboard Trail, Camelback Road and Brown Avenue, Buckboard Trail and Shoeman Lane, Brown Avenue and Shoeman Lane. Camelback Road is signalized at the intersections with Scottsdale Road and Miller Road.

For the section of Camelback Road from Scottsdale Road to Miller there were twelve collisions reported in 2002; the collision rate for this segment of roadway was 2.80, higher than the city average rate of 1.49.

At the signalized intersection of Camelback Road and Scottsdale Road, there were 19 collisions reported in 2002; the collision rate for this intersection was 0.90; higher than the city average rate of 0.54. There were 13 reported collisions at this intersection in 2003, and 13 reported in 2004 through September.

At the intersection of Camelback Road and Buckboard Trail, there were two reported collisions in 2002, two in 2003, and seven in 2004 through September. At the intersection of Camelback Road and Brown Avenue, there was one collision reported in 2002, and one in 2004 through September.

Proposed Development:

The approximately 2.2-acre site currently has a mixture of three downtown zoning designations: Highway Commercial, Downtown Overlay District (C-3 DO); Central Business, Parking, Downtown Overlay District (C-2 P-3 DO); and Automobile Parking, Downtown Overlay District (P-2 DO). The request is to rezone the site to Downtown/Office Residential District Type 2, Planned Block Development, Downtown Overlay (D/OR-2 PBD DO) with amended development standards. The proposed zoning would allow the development of mixed-use project consisting of a hotel, some residential units, a restaurant and bar, and some minor retail. The trip generation numbers for proposed development plan are presented in the Table 1 below.

TABLE 1 -Trip Generation for Proposed Development Plan

| Land Use | Daily Total | AM Peak Hour | | | PM Peak Hour | | |
|---------------------------------|--------------|--------------|-----------|------------|--------------|------------|------------|
| | | In | Out | Total | In | Out | Total |
| Hotel – 225 Rooms | 1,404 | 72 | 36 | 108 | 52 | 72 | 124 |
| Condominiums – 25 units | 198 | 3 | 14 | 17 | 13 | 6 | 19 |
| Restaurant – 3,125 s.f. | 281 | 2 | 1 | 1 | 16 | 7 | 23 |
| Bar – 1,500 s.f. | 213 | 0 | 0 | 0 | 11 | 6 | 17 |
| Retail – 3,000 s.f. | 124 | 0 | 0 | 0 | 11 | 15 | 26 |
| Subtotal | 2,220 | 77 | 51 | 128 | 103 | 106 | 209 |
| 5% Reduction for Internal Trips | 111 | 3 | 3 | 6 | 5 | 5 | 10 |
| 5% Reduction for Other Modes | 111 | 3 | 3 | 6 | 5 | 5 | 10 |
| Total Trips | 1,998 | 71 | 45 | 116 | 93 | 96 | 189 |

This trip generation is based on data contained in the Institute of Transportation Engineer's *Trip Generation*. A traffic impact study was prepared by Parsons Brinckerhoff under the City's Traffic Impact and Mitigation Analysis (TIMA) guidelines. The study compares the trip generation characteristics of the proposal versus the previously approved site plan and examines the impacts from the proposed development. A five percent reduction was applied to the site trip generation to account for the interaction between the various land uses proposed for the site – hotel, residential, restaurant, and retail. In other words, it is likely that some of the hotel guests and condominium residents will patronize the commercial uses on the site. In addition, a five percent reduction was applied to account for trips that will utilize non-vehicular modes of transportation. This is very likely considering that the site is located in an urbanized area of the city, with transit, trolley, and pedestrian amenities being provided in the vicinity of the site. This also

reflects the ability of the hotel guests and condominium residents to walk to the nearby commercial land uses (Fashion Square Mall, Scottsdale Waterfront, downtown restaurants, etc.).

The trip generation numbers for development that would be allowed under the existing zoning are presented in Table 2 below. These trip generation numbers are based on a land use mixture of general office and restaurant, similar to the prevalent land uses in the surrounding area. A comparison of the trips generated by the proposed development plan versus development under the current zoning is shown in Table 3.

TABLE 2 - Trip Generation for Development Under Current Zoning

| Land Use | Daily Total | AM Peak Hour | | | PM Peak Hour | | |
|-------------------------|-------------|--------------|-----------|-----------|--------------|------------|------------|
| | | In | Out | Total | In | Out | Total |
| Restaurant – 7,280 s.f. | 654 | 6 | 6 | 23 | 73 | 36 | 109 |
| Office – 14,500 s.f. | 302 | 35 | 5 | 40 | 16 | 79 | 95 |
| Total | 956 | 41 | 11 | 63 | 89 | 115 | 204 |

TABLE 3 - Comparison Trip Generation

| Land Use | Daily Total | AM Peak Hour | | | PM Peak Hour | | |
|---------------------------|---------------|--------------|------------|------------|--------------|------------|------------|
| | | In | Out | Total | In | Out | Total |
| Current Zoning | 956 | 41 | 11 | 63 | 89 | 115 | 204 |
| Proposed Development Plan | 1,998 | 71 | 45 | 116 | 93 | 96 | 189 |
| Change | +1,042 | +30 | +34 | +53 | +4 | -19 | -15 |

The comparison trip generation in Table 3 indicated an estimated increase of approximately 1,000 daily vehicles under the proposed development plan. The change from more strictly commercial land uses to a mixture of hotel, residential, and commercial land uses results in more a.m. peak hour traffic and less p.m. peak hour traffic.

Site access will primarily be provided via a proposed driveway on Camelback Road between Buckboard Trail and Brown Avenue. Secondary site access for the hotel and commercial uses is proposed on Buckboard Trail. Access for the condominium units is proposed to be separated from the hotel entrance, and will be located either Brown Avenue or Shoeman Lane. All on-site parking is proposed to be located in an underground parking structure. There are some public on-street parking spaces located on Buckboard Trail, Brown Avenue, and Shoeman Lane.

Future Conditions:

The submitted traffic study analyzes the traffic conditions for the Year 2010. Future traffic volumes on the adjacent streets were calculated by applying annualized growth rates to the existing traffic volumes. Traffic volume projections from the Scottsdale Waterfront Project were also included. Capacity calculations were performed using three sets of data: existing traffic volumes, 2010 projected traffic volumes (background traffic), and 2010 projected traffic volumes plus site generated traffic (total traffic).

A summary of the results of the capacity analyses for the study intersections is shown in Table 4 below. At the three unsignalized intersections along Camelback Road, the

westbound left turns from Camelback Road and the northbound right turns onto Camelback Road all operate at LOS C or better. The northbound left turns onto Camelback Road operate at a poor level of service in the p.m. peak hour during 2010 conditions; this is typical for unsignalized intersections along arterial streets. The street network in the vicinity of the site provides other options for drivers desiring to head westbound on Camelback Road. Providing a connection from the main site entrance on Camelback Road to Buckboard Trail allows the patrons of the hotel, restaurant, and retail uses to use these alternative routes.

The signalized intersection of Camelback Road and Scottsdale Road operates at LOS D in the a.m. peak hour and LOS E in the peak hour under 2010 conditions with and without the site generated traffic. The additional site generated traffic does not degrade the level of service at the intersection. The capacity calculations for the intersection of Camelback Road and Scottsdale Road assume that a dual eastbound left-turn will be provided which is being planned as part of the Scottsdale Waterfront Project infrastructure improvements.

TABLE 4
Signalized Intersections Capacity Analyses
Level of Service/Average Control Delay (in seconds)

| Unsignalized Intersections | Existing Cond.'s 2004 Volumes | | Background Traffic 2010 Volumes | | Total Traffic 2010 Volumes | |
|--------------------------------|----------------------------------|-----------|---------------------------------------|-----------|-------------------------------|-----------|
| | AM | PM | AM | PM | AM | PM |
| Camelback/Brown | | | | | | |
| WB Left | A | B | A | B | A | B |
| NB Right | A | B | B | C | B | C |
| NB Left | C | E | D | F | D | F |
| | | | | | | |
| Camelback/Buckboard | AM | PM | AM | PM | AM | PM |
| WB Left | A | B | A | B | A | C |
| NB Right | A | B | B | C | B | C |
| NB Left | C | E | D | F | D | F |
| | | | | | | |
| Camelback/Site Driveway | AM | PM | AM | PM | AM | PM |
| WB Left | N/A | N/A | N/A | N/A | A | B |
| NB Right | N/A | N/A | N/A | N/A | B | C |
| NB Left | N/A | N/A | N/A | N/A | C | F |
| | | | | | | |
| Signalized Intersection | | | | | | |
| Camelback/Scottsdale | AM | PM | AM | PM | AM | PM |
| EB Approach | D | E | D | F | D | F |
| WB Approach | E | D | F | E | F | E |
| NB Approach | C | D | C | E | C | E |
| SB Approach | C | D | C | D | C | D |
| Intersection | D | D | D | E | D | E |

* assumes dual left turn lanes eastbound on Camelback

Summary:

The approval of the proposed development plan under the proposed zoning district and amended development standards will generate an estimated 1,998 trips per day, with approximately 116 trips generated in the a.m. peak hour and 189 trips generated in the p.m. peak hour. This represents an increase of approximately 1,000 daily trips when compared to the estimated traffic that would be generated if the site were developed as a mixture of restaurant and office land uses under the existing zoning. There is a slight increase in a.m. peak hour traffic (53 trips) and a slight decrease in p.m. peak hour traffic (15 trips).

The proposed mixture of hotel, residential, restaurant, and retail land uses will reduce the amount of site-generated traffic due to the natural interaction of these land uses. The site location will encourage the use of alternative modes of transportation for those traveling to the site and for those residents who live on the site.

Site traffic will be distributed primarily to Camelback Road via the unsignalized intersections of Buckboard Trail, Brown Avenue, and the proposed site driveway. Camelback Road is under capacity for current and projected traffic volumes.

Capacity analyses for the Year 2010 indicate that the most movements at unsignalized intersections along Camelback Road in the vicinity of the site will operate at level of service C or better. These study intersections include Buckboard Trail, Brown Avenue, and the proposed site driveway. As is typical for unsignalized intersections on arterial streets, the left-turn movements onto Camelback Road operate at poor level of service at all of these intersections. The surrounding street network does provide alternative routes for drivers desiring to head westbound on Camelback Road.

Staff Concerns/Comments:

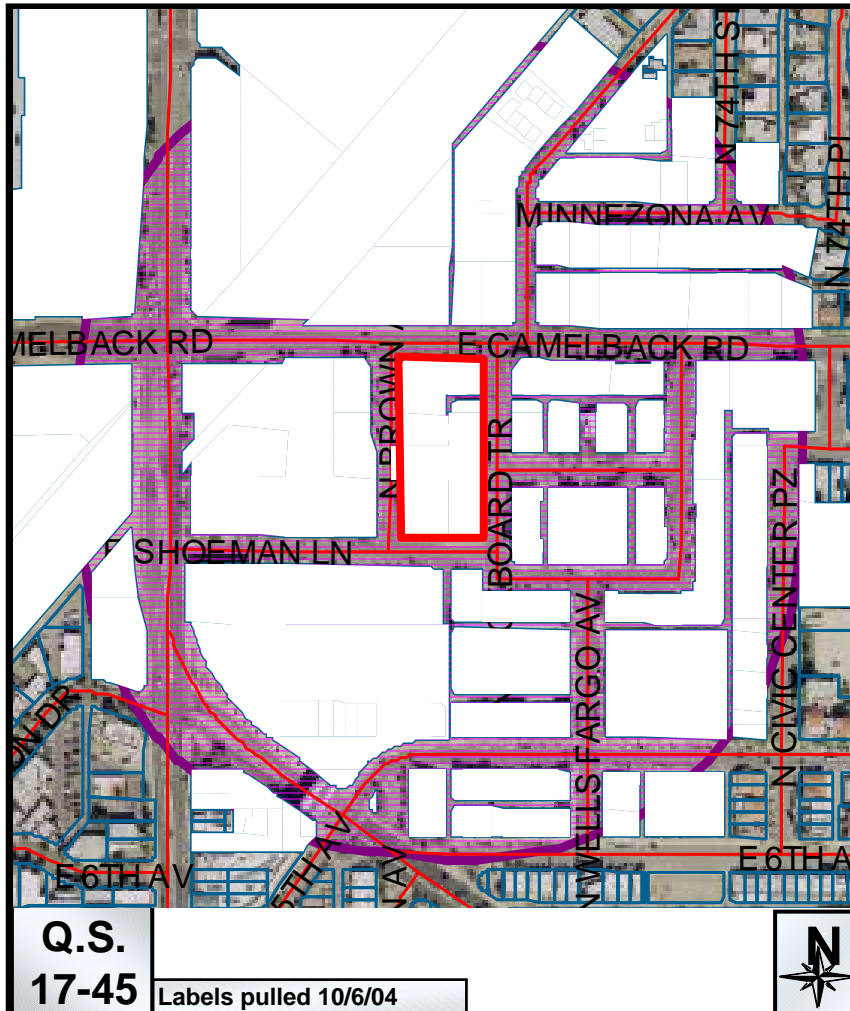
- The proposed site driveway on Camelback Road does not meet minimum intersection spacing criteria for minor arterial streets. The driveway is located approximately 135 feet from the adjacent Buckboard Trail and Brown Avenue intersections.
- The development plan should provide enhanced pedestrian connections to the adjacent land uses.

W Hotel Scottsdale
22-ZN-2004 & 19-AB-2004

Attachment #9. Citizen Involvement

The above attachment is on file at the City of
Scottsdale Current Planning office,
7447 E Indian School Road, Suite 105.

City Notifications – Mailing List Selection Map



Map Legend:



Site Boundary



Properties within 750-feet

Additional Notifications:

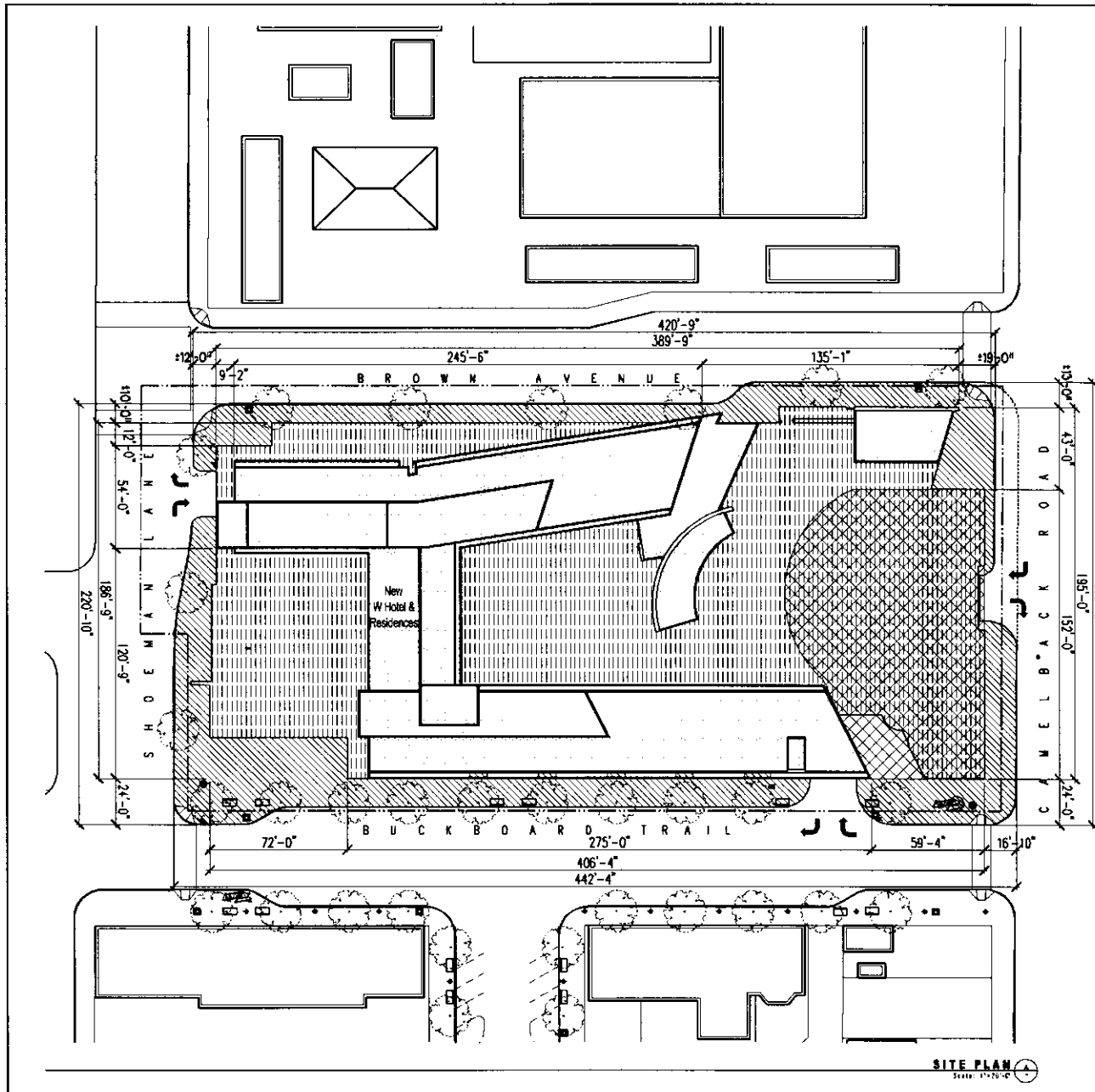
- Interested Parties
- South Scottsdale Redevelopment Force
- The Heritage - Scottsdale Owners Association

W Hotel Scottsdale

22-ZN-2004

ATTACHMENT #10

L:\proj\018492_Civil\Thumbnails\Attachments\1_FullSize\WHS_A0101A.dwg, 10/19/2024 05:47:54 PM, written: 2004 EPDS-100-PT06.pct, ANSYS B (11.00 x 17.00 inches)



Site Plan Worksheet

Site Area Per ALTA Survey: 95,148 SQ FT
Total Site Area: 128,273 SQ FT (including Dedications prior to 1987)

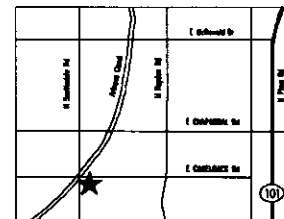
F.A.R. Allowable Area: = 204,381 SQ FT
Total Building Area: 198,107 SQ FT

Total Parking Required: 459 (294 with credits)
Total Parking Provided: 459 (217 on site)

Building Height: 72 Feet

Site Plan Legend

Denotes Street Level Open Space: [Pattern]
Denotes Podium Level Open Space: [Pattern]
Denotes Porte Cochere Open Space: [Pattern]



LOCATION MAP

TRIYAR
Hospitality LLC
2803 North
14th Street
Suite 150
Phoenix
AZ 85008

**W
SCOTTSDALE**
Hotel + Residences
schematic design
documents

SITE PLAN

Hornberger
+
Worstell

Hornberger + Worstell Inc.
Architects & Planners
170 Walter Lane
San Francisco
California
94110
www.hornbergerworstell.com

ISSUED
10.19.24
PLANNING SUBMITTAL

For use in connection with the project shown on this drawing, the user agrees to indemnify and hold the architect harmless from all claims, damages, and expenses, including reasonable attorney's fees, arising from the use of this drawing for any purpose other than that for which it was prepared.

